



# PORT OF MELBOURNE

2020 Container Logistics Chain Study Launch  
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Port of Melbourne





# KEY FINDINGS

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Port of Melbourne



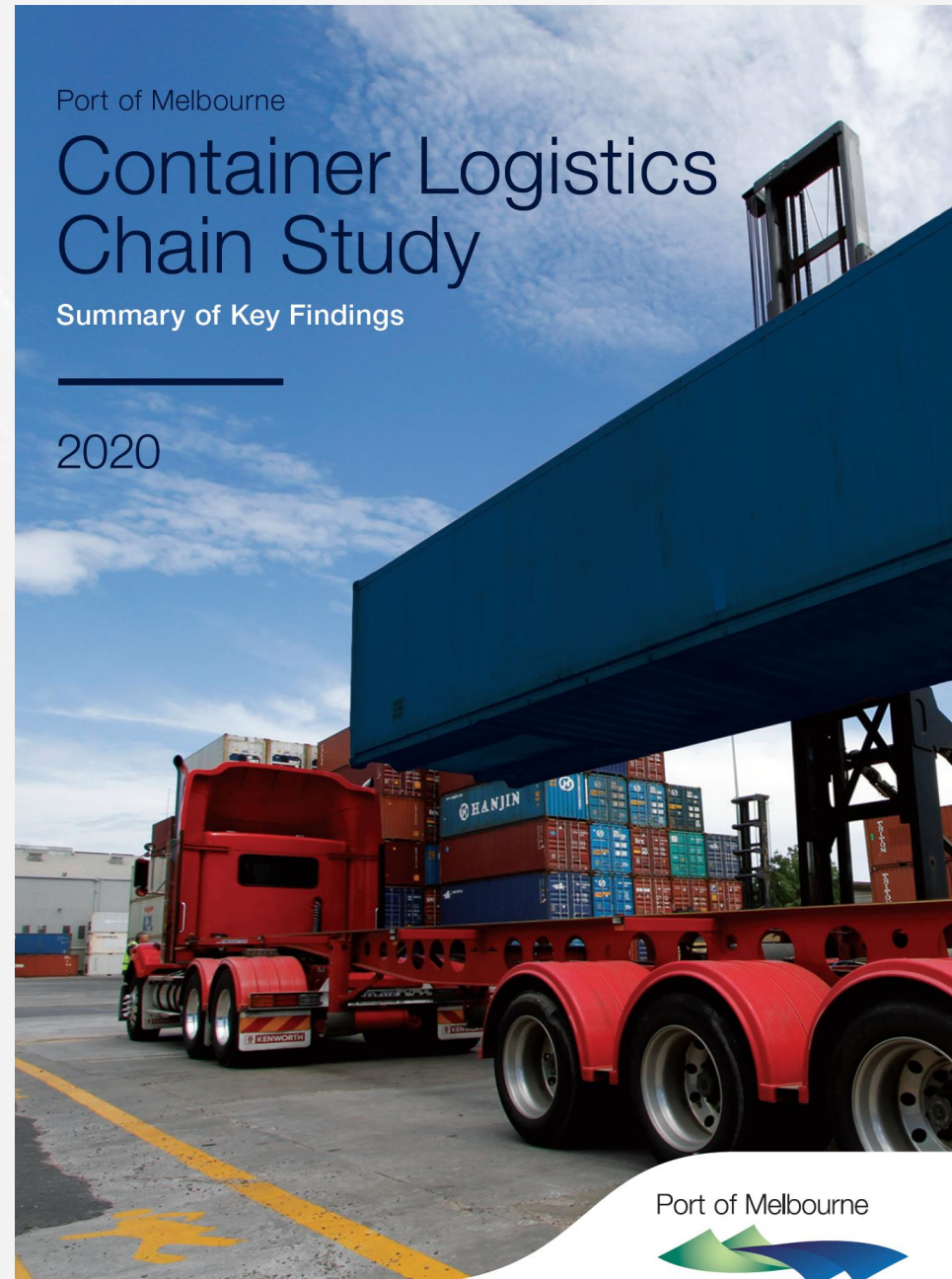
# Study overview

Port of Melbourne commenced the study in 2019

It builds on the CLCS study of 2009.

GHD Advisory undertook the study, in conjunction with the Department of Transport.

The full report by GHD and a summary report developed by PoM will be available on the PoM website  
[www.portofmelbourne.com](http://www.portofmelbourne.com)



# Key Finding – Import Containers

## International & Mainland full import containers

94% to Metropolitan Melbourne

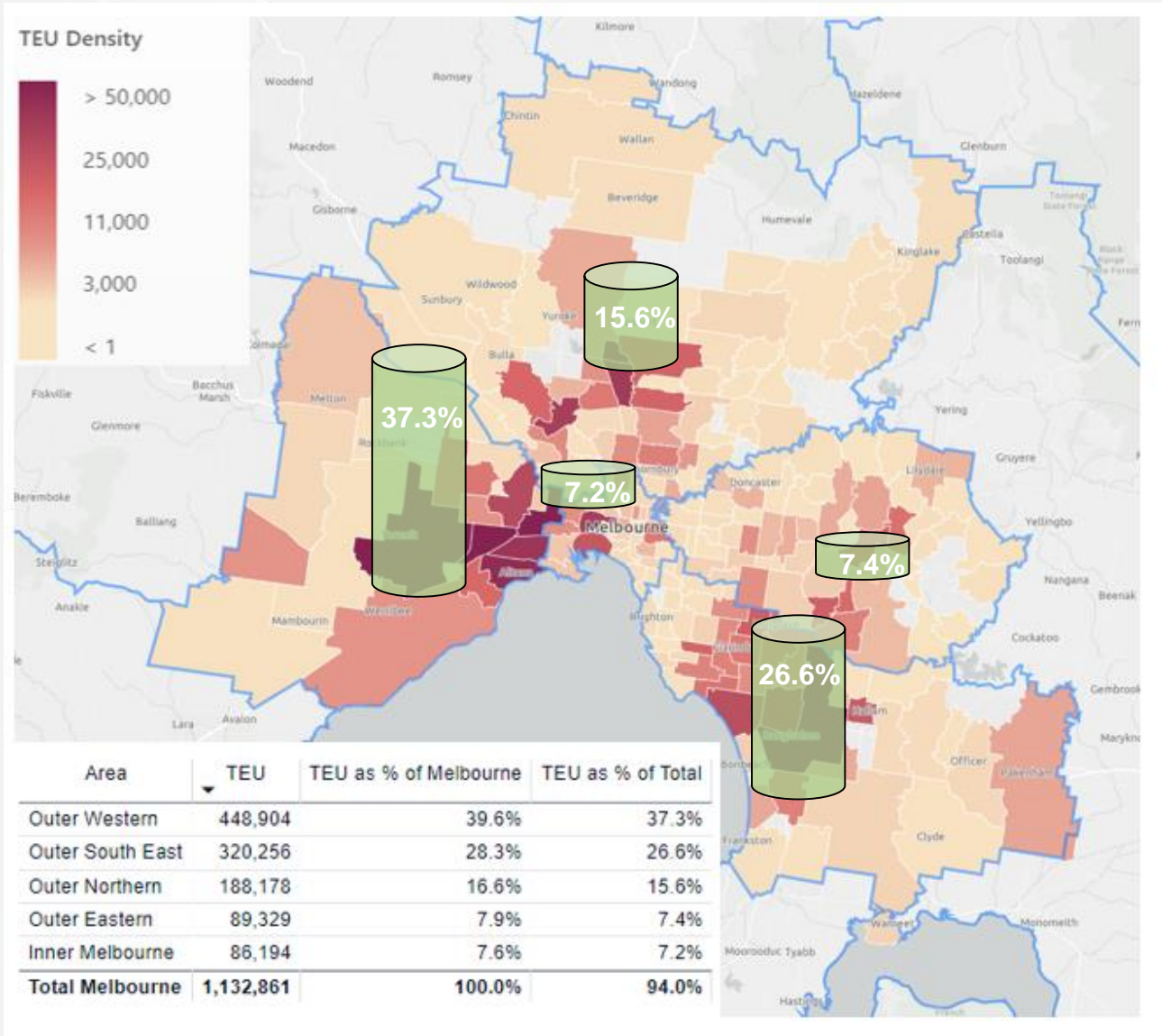
3.8% to Regional Victoria

1.5% to New South Wales and ACT

Balance (0.7%) to other States

### Largest postcodes

1. Derrimut – 142,223 TEU (11.8%)
2. Dandenong South – 142,195 TEU (11.8%)
3. Truganina – 128,214 TEU (10.6%)
4. Brooklyn – 50,319 TEU (4.2%)
5. Altona – 43,082 TEU (3.6%)



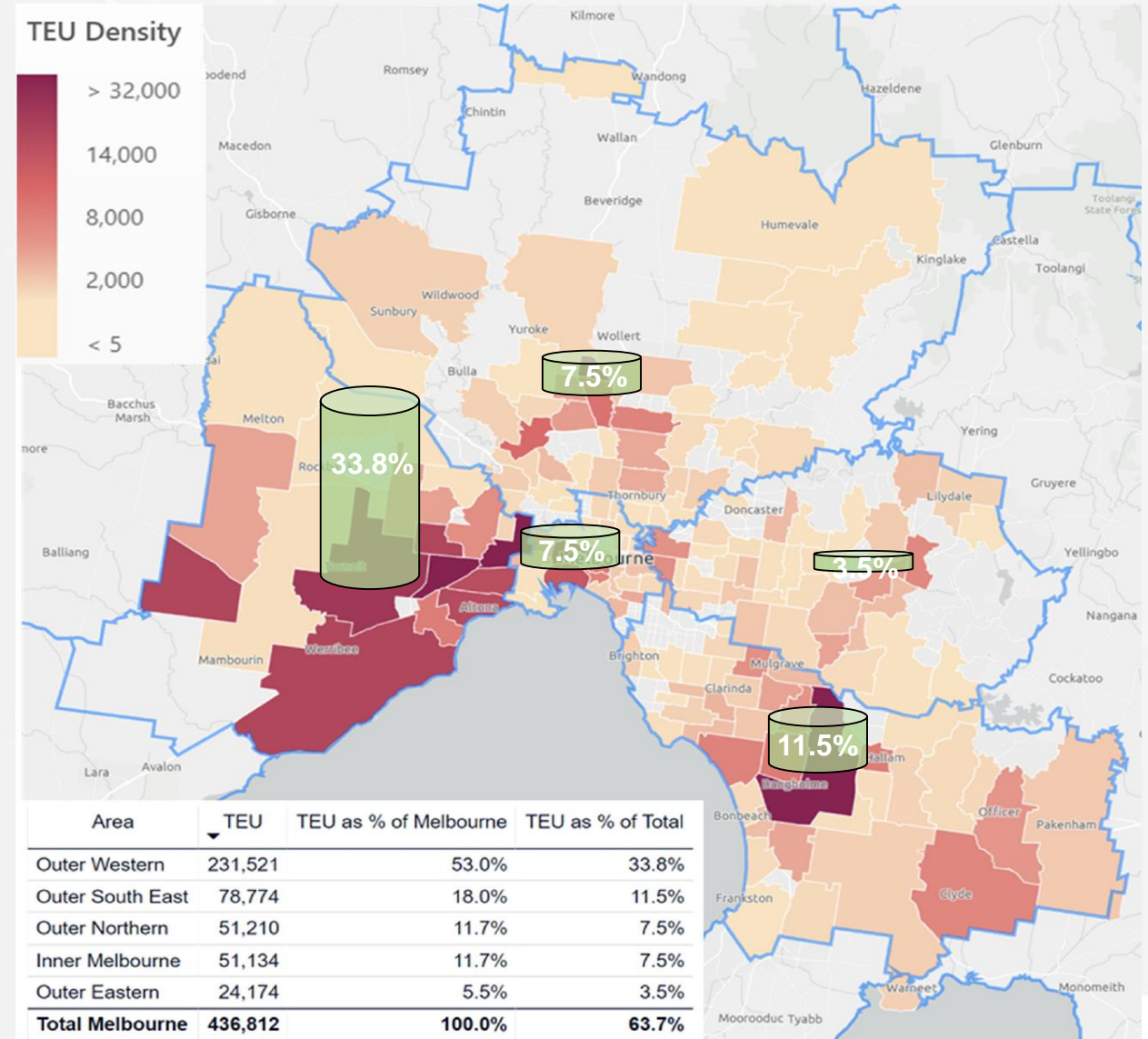
# Key Finding – Export Containers – Metropolitan Melbourne

## International & Mainland full export containers

64% from Metropolitan Melbourne

### Largest postcodes

1. Brooklyn – 87,217 TEU (12.7%)
2. Derrimut – 71,574 TEU (10.4%)
3. Dandenong South – 35,260 TEU (5.1%)
4. Truganina – 20,921 TEU (3.1%)
5. West Melbourne – 18,989 TEU (2.8%)

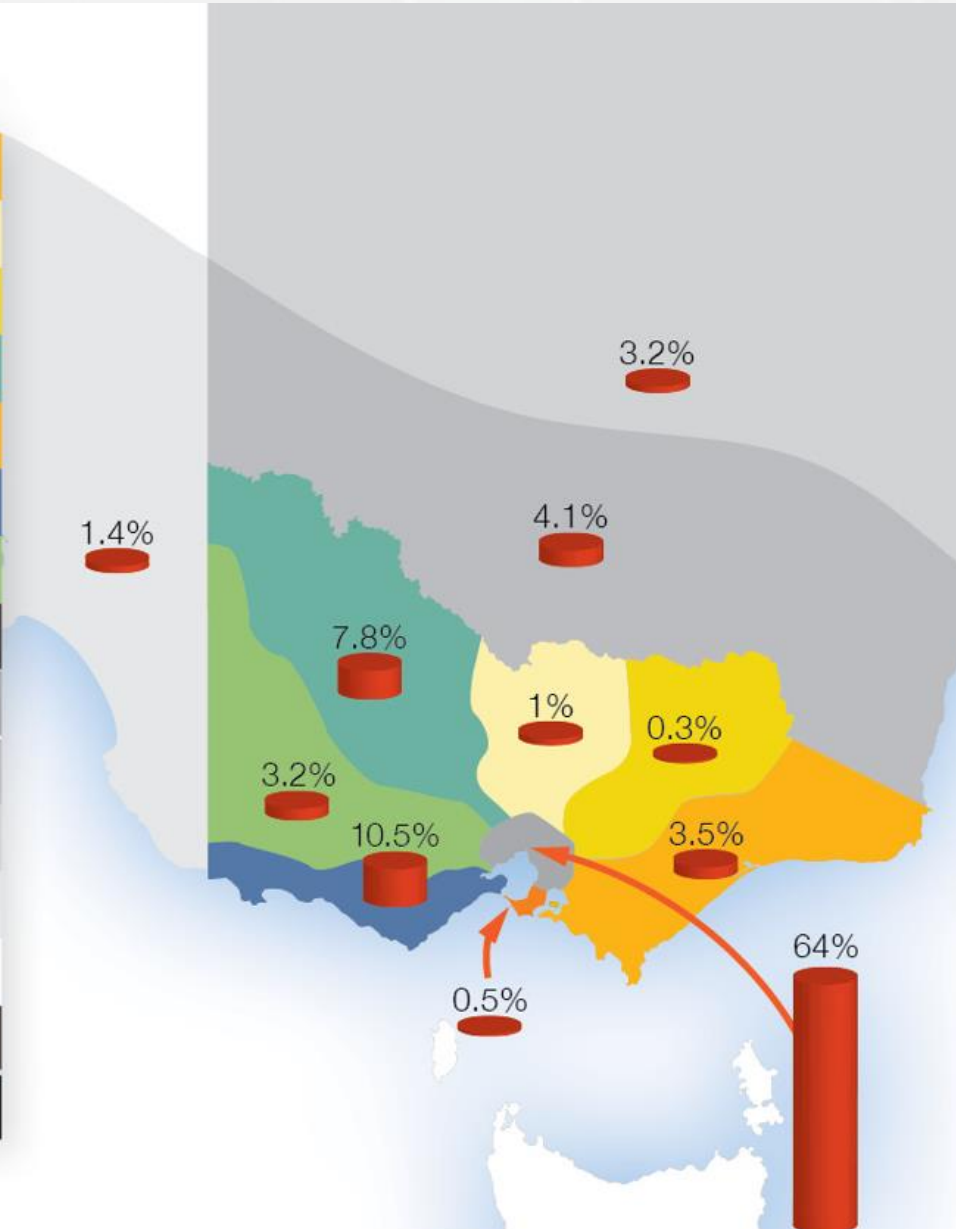


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# Key Finding – Export Containers – Regional Victoria & Interstate

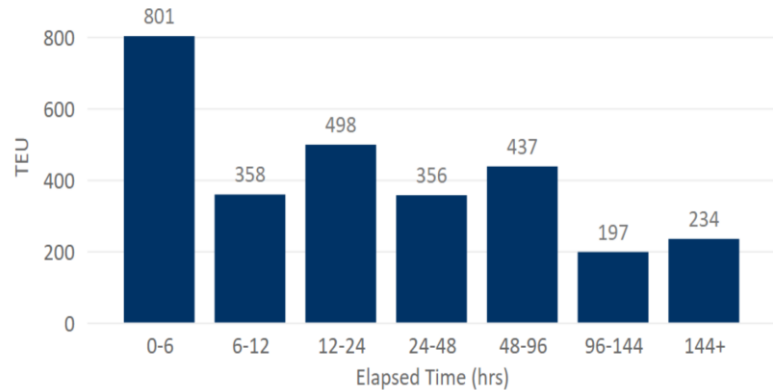
Area	TEU	%
Eastern Corridor	24,122	3.5%
Goulburn Corridor	6,940	1.0%
Hume Corridor	2,216	0.3%
North Western Corridor	53,526	7.8%
Peninsula	3,653	0.5%
South Western Corridor	71,708	10.5%
Western Corridor	22,073	3.2%
<b>REGIONAL VICTORIA TOTAL</b>	<b>184,238</b>	<b>27%</b>
<b>Metropolitan Total</b>	<b>436,813</b>	<b>64%</b>
Southern NSW & ACT	28,357	4.1%
Remainder of NSW	22,147	3.2%
South Australia	9,621	1.4%
Other states	4,076	0.6%
<b>INTERSTATE TOTAL</b>	<b>64,201</b>	<b>9.4%</b>
<b>GRAND TOTAL</b>	<b>685,252</b>	<b>100%</b>



# Key Finding – Staging

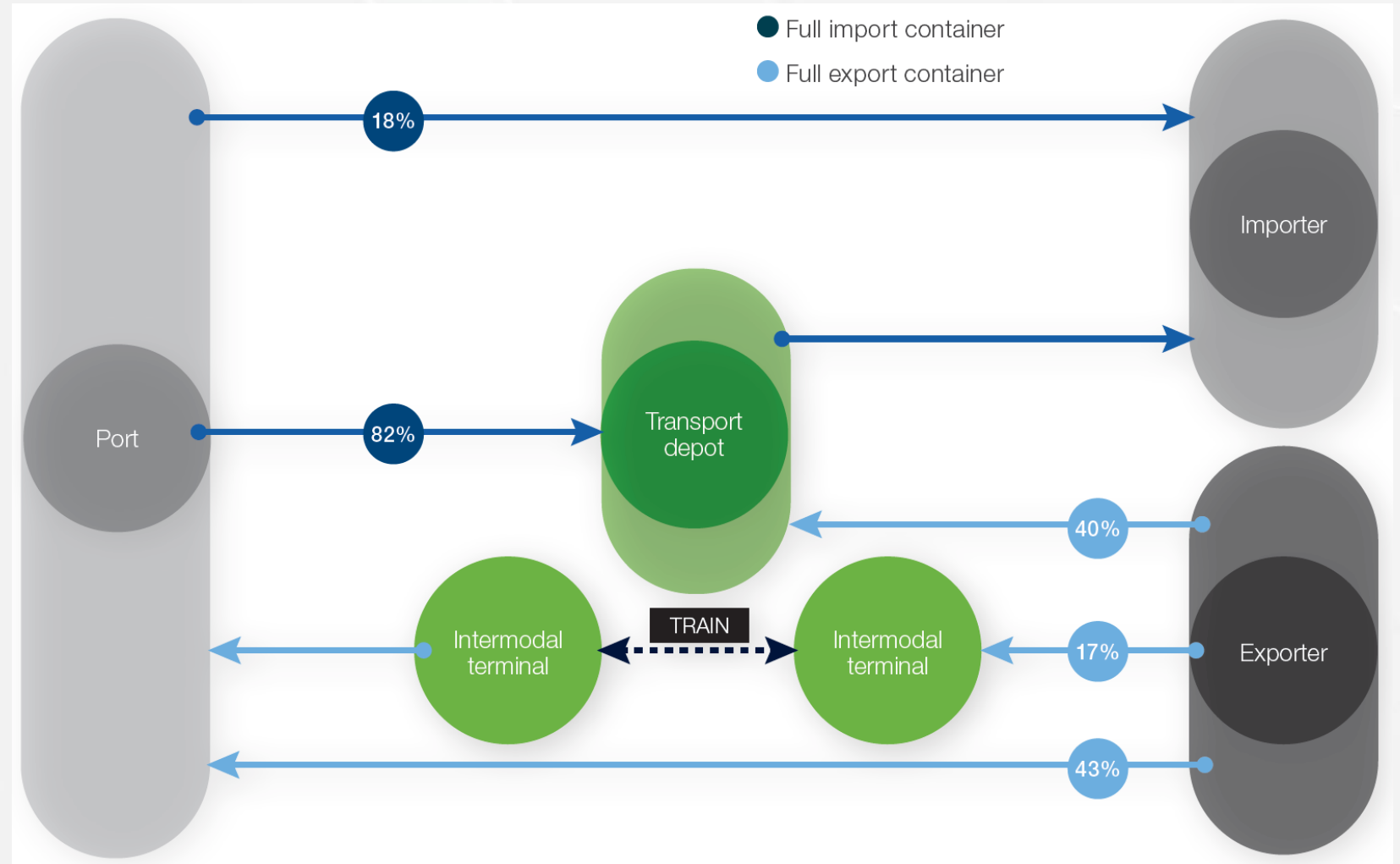
82 % of full import containers were staged at a Transport Depot

- 40% of these were staged for less than 12 hours
- 57.5% staged for less than 24 hours
- Average time in depot – 1.9 days



40% of full export containers were staged at a Transport Depot and 17% were staged at rail intermodal terminals.

Transport companies are providing the bridge between 24/7 port times and preferred customer delivery/pick up time.

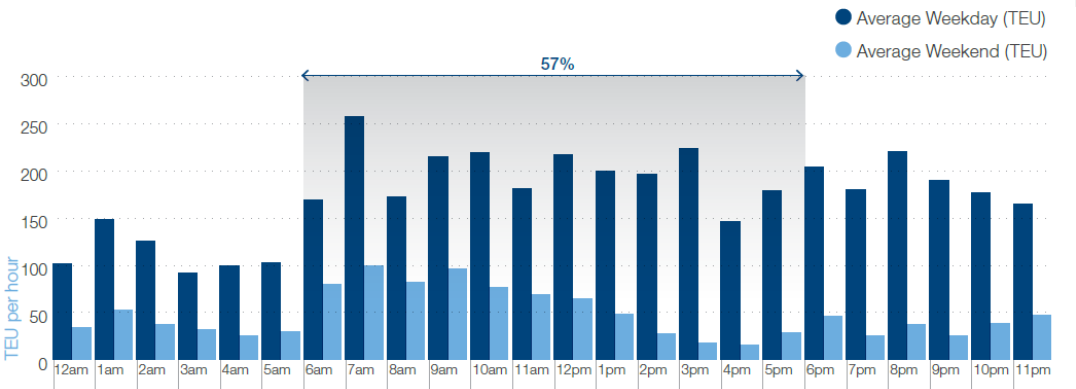


# Key Finding – Operating Times

## PORT

Departure time of full International & Mainland import containers from Stevedores

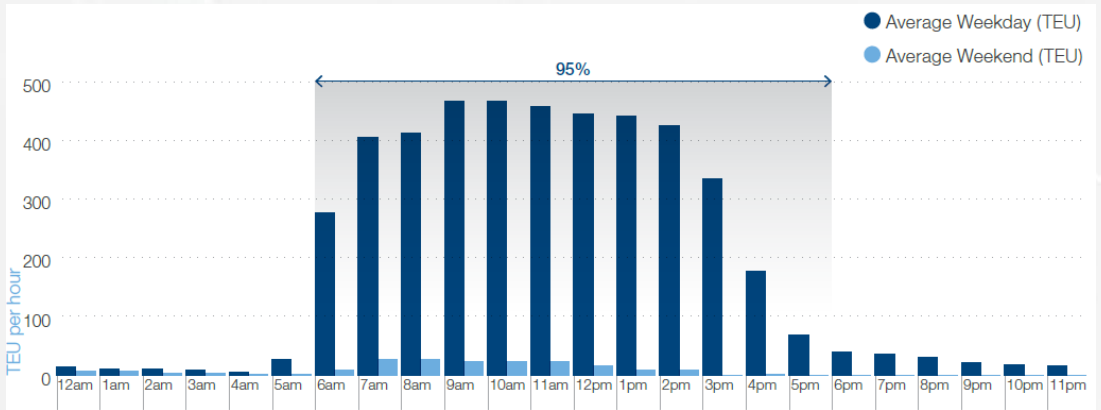
43% picked up between 6 PM and 6 AM



## CONTAINER PARKS

Empty container arrivals to empty container parks

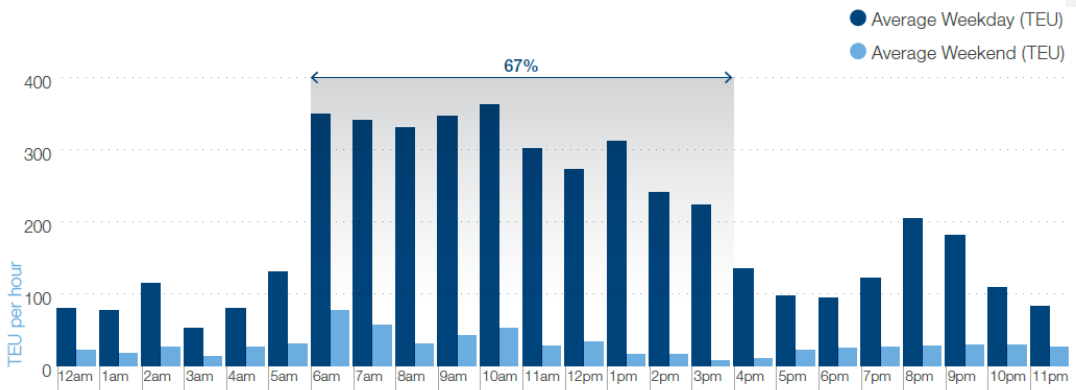
Around 95% arrived between 6 AM and 6 PM



## IMPORTERS

Arrival time of full International & Mainland import containers at importers

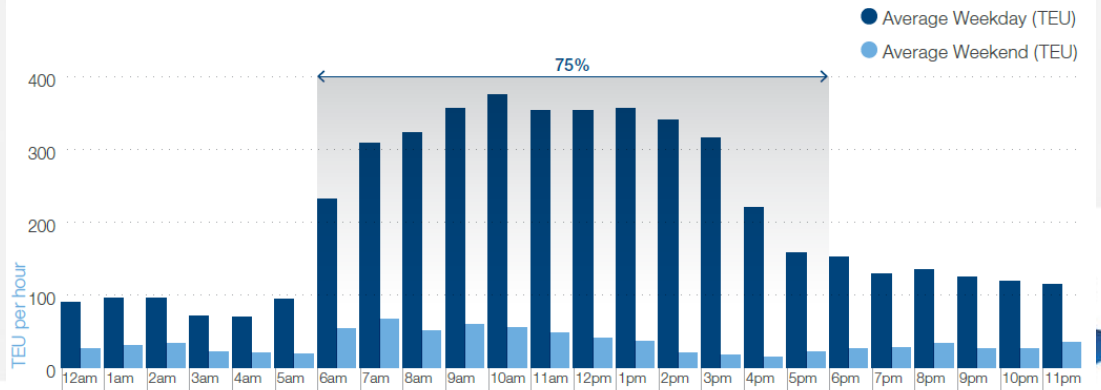
Less than 30% arrived between 6 PM and 6 AM



## TRANSPORT DEPOTS

Arrival time of full International & Mainland import and export containers at transport depots

Approximately 75% arrived between 6 AM and 6 PM





# Changes 2019 vs 2009

The 2019 CLCS report follows a similar format to the 2009 report allowing an accurate comparison of the key results.

Metric	2019	2009		% Change
Use of forty foot containers	56%	39%	↑	44%
Import Destination (full) – Metropolitan Melbourne	94%	87%	↑	8%
Import Destination (full) – Western Suburbs	37%	26%	↑	41%
Import Destination (full) – Northern Suburbs	16%	19%	↓	-19%
Export Origin (full) - Interstate	9%	23%	↓	-59%
Staging Imports	82%	71%	↑	16%
Rail Mode Share	8%	14%	↓	-55%
Road HPFV Share	24%	13%	↑	85%



# Strategic Considerations

## Key observation #1

- The centre of activity for container movements remains in the west of Melbourne, and is likely to continue to grow. This supports WIFT more than BIFT for location.

## Key observation #2

- Strategic Transport Planning to meet freight needs is necessary across Metropolitan Melbourne. Greater use of HPFV and dynamic load limits on key bridges required.

## Key observation #3

- Amenity of residents in inner Melbourne needs to be considered as part of the growth in container movements in western Melbourne.

## Key observation #4

- Rail mode share has declined significantly since the last study.



# Actions

- **Rail**

- Support the increased use of rail transport for Port containers
- Support development of the Webb Dock rail link
- Connect WIFT directly to the Port via a dedicated freight link
- Progress development of port rail shuttle network

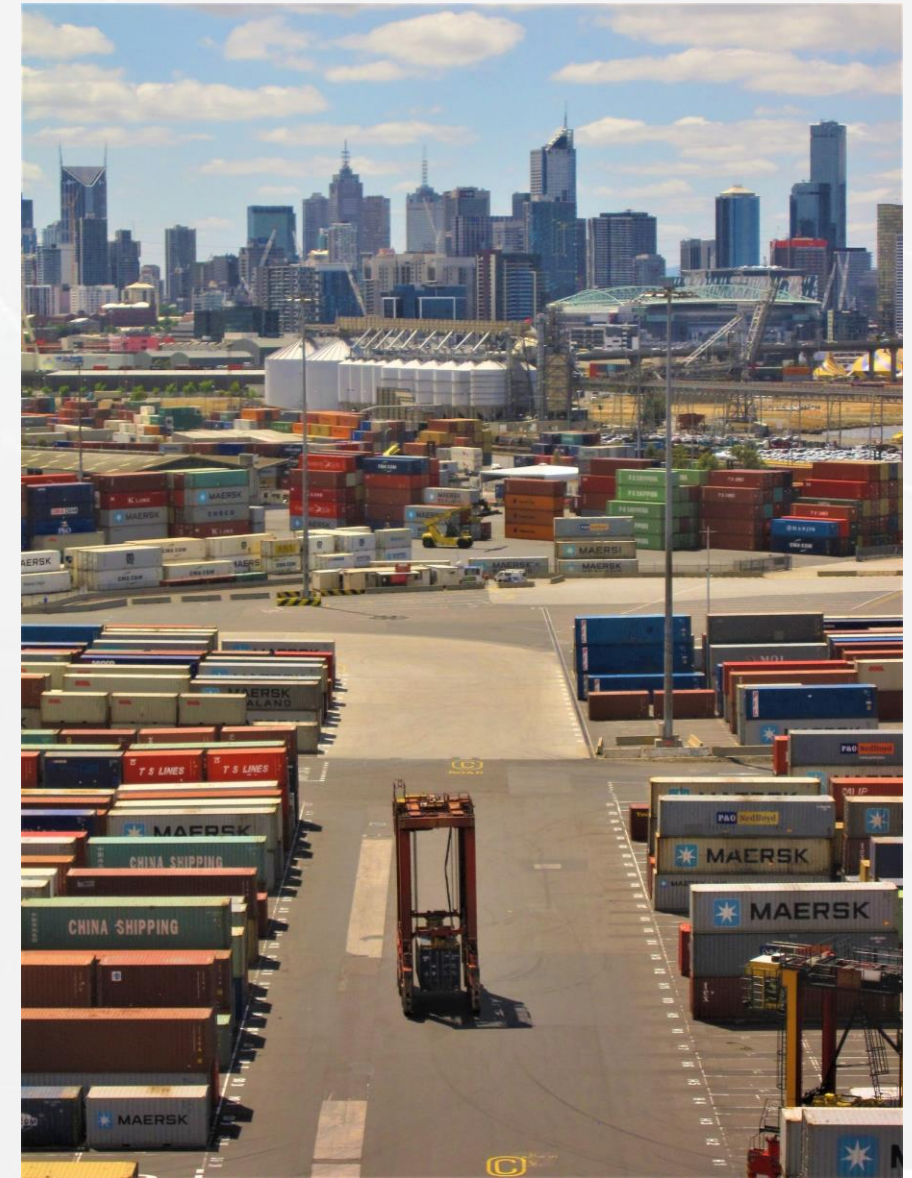
- **Road**

- Expand the HPFV network to include direct links to high volume import/export zones
- Promote the use of off peak capacity on the arterial road network
- Identify opportunities for direct transport between the Port and importers/exporters outside of regular business hours



# Actions (continued)

- **Land use and Transport Planning**
  - Promote the relocation of transport depots to freight precincts in outer metro areas
  - Ensure efficient freight movements is supported through transport and land use frameworks
  - Ensure future planning for growth of greater Melbourne includes appropriate freight corridors
  - Update strategic planning instruments to protect industrial land zoning.



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# Thank you

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