



PORT LOAD CHART

About the Port Load Chart

The Port Load Chart is intended to provide guidance on the maximum allowable loads permitted on wharves, piers and shed floors at PoM owned facilities within the Port of Melbourne. The details are correct at time of publication but may be subject to variation.

Revisions and Updates

The Port Load Chart will be revised and updated on a regular basis. The latest version will be on the Port of Melbourne Operations web site www.portofmelbourne.com. It is the responsibility of persons using this chart to ensure that they are referring to the latest version.

In this revision, PoM contact details and the allowable load for the following wharves has been updated: Webb Dock East 3, Gellibrand Pier, Appleton Dock B to E, Yarraville 6, South Wharf 27 and Short Road Wharf & Tug Berths.

Load Categories

The Port Load Chart is only applicable to the standard load categories defined below and in the Standard Loads Diagram – Attachment 1.

Single Axle Load

The maximum single axle load (except fork lifts) is shown in Column A of the Chart. Axles must be a minimum of 2.4 m apart to be considered single axles. (Note: VicRoads maximum 9 tonne for standard road registered vehicles)

Tandem Axle Group Load

The maximum tandem axle group load is shown in Column B of the Chart. Axles must be a minimum of 1.2 m apart within the group. (Note: VicRoads maximum 16.5 tonne for standard road registered vehicles)

Tri-Axle Group Load

The maximum tri-axle group load is shown in Column C of the Chart. Axles must be a minimum of 1.2 m apart within the group. (Note: VicRoads maximum 20 tonne for standard road registered vehicles)



Forklift Front Axle Load

The maximum load on a forklift front axle is shown in Column D of the Chart. Forklifts may exceed the loads shown in Column A, provided the following conditions are met:

- The forklift keeps at least 6 m from all other vehicles except the one that is being loaded.
- The maximum axle load does not exceed the amount shown in Column D.

Uniformly Spread Load

The maximum uniformly spread load from cargo stacking over a large area is shown in Column E. This may be exceeded over a small area provided the equivalent axle group load is not exceeded.

Cranes

The maximum crane outrigger loads, with load spreading pads 1 m x 1 m, shall not exceed half of the maximum load detailed in Column D. Higher loads may be approved for larger approved load spreading devices, refer to Non-Standard Loads.

General

The above axle loads are based on dual tyred wheels (i.e., 4 tyres per axle). Where single tyred wheels (i.e. 2 tyres per axle) are used, then axle loads shall not exceed the registered load capacity.

Axle loads include the weight of wheels etc. Where spacing's are less that the above limits, allowable loads shall be reduced proportionately.

Non-Standard Loads

The above loads are standard design loads for the respective areas. For further information, including the non-standard loads approval process, please goto: <https://www.portofmelbourne.com/facilities-development/use-of-port-facilities/> and email details to nstandload@portofmelbourne.com. At least 2 weeks' notice is normally required for non-standard load approval. However, 4 weeks' notice may be needed for more complex loads that require a higher level of planning. Typically the complexity cannot be determined until after the assessment has started.

Port Load Charts

The maximum standard loads permitted on wharves, piers and shed floors are shown in the following Port Load Chart.



BERTH No./LOCATION	COL. A	COL. B	COL. C	COL. D	COL. E
	SINGLE AXLE (T)	TANDEM AXLE (T)	TRI- AXLE (T)	FORK LIFT (T)	SPREAD LOADS (T/SQ.m)
SWANSON DOCK East/West					
All Berths	32	52	52	100	4.5
VICTORIA DOCK					
Berth 24 Wharf apron	20	32	48	50	2.5
Berth 24 Shed floor	14	24	30	20	3
SOUTH WHARF					
24 Wharf loading ramp Except 2.5 m from the nib's edge (between chainage 244 to 241 m & 203 to 206 m) which cannot accept any live load	14	24	30	20	4
26 Wharf (Chainage 515 m to 792 m)					
• Wharf apron chainage 515 to 590 m	6	10	14	6	0.5
• Wharf apron chainage 590 to 792 m	9	16.5	20	20	1
• Raised concrete deck, landings, berth dolphins	14	16.5	16.5	14	1.25
27 Wharf (Chainage 0 m to 210 m)					
• Wharf apron	9	16.5	20	20	1
• Shed Floor	9	16.5	20	20	3
28 Wharf (Chainage 210 m to 293 m)					
• Within 6 m of wharf face	2	2	2	2	NIL
• Between 6 m to 14 m of wharf face	16	20	30	20	1.25
• Former shed floor	16	20	30	20	3
29 Wharf (Chainage 293 m to 440 m)					
• Within 6 m of wharf face	2	2	2	2	NIL
• Between 6 m to 14 m of wharf face	16	20	30	20	1.25
• Former shed floor	16	20	30	20	3
30 Wharf (Chainage 440 m to 632 m)					
• Apron-rear section	9	16.5	NIL	14	0.5
• Shed floor	16	20	30	20	3
31 Wharf (Chainage 632 m to 759 m)					
• Apron	9	16.5	NIL	14	0.5
• Shed floor	9	16.5	20	14	1.5
32 Wharf apron	9	16.5	20	20	1.5
33 Wharf	20	32	36	20	1
NO. 1 MARIBYRNONG					
Pier head	4.2	10	15	5	0.4
Vehicular approaches	9	16.5	20	9	1.0
ANN STREET PIER					
• Main Wharf (from pile bent 1 to 44 only, except restricted area along the wharf). No load after pile bent 44 (pedestrian only).	9	16.5	20	13	1
• Cross Wharf (general area including behind sheet pile wall)	9	NIL	NIL	9	1
• Cross Wharf (New Hardstand)	9	16.5	20	35	1.5
• Small Boat Jetty	NIL	NIL	NIL	NIL	0.5



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Page
4 of 6

BERTH No./LOCATION	COL. A	COL. B	COL. C	COL. D	COL. E
	SINGLE AXLE (T)	TANDEM AXLE (T)	TRI- AXLE (T)	FORK LIFT (T)	SPREAD LOADS (T/SQ.m)
APPLETON DOCK					
B, C, D & E Appleton Dock Wharves - No load within the following areas; <ul style="list-style-type: none"> Front 1.8 m width of wharf, with the exception of bulk cargo hoppers which may be located a minimum of 1.2 m from the wharf edge. 					
B & C Appleton Dock (tired vehicles only)					
<ul style="list-style-type: none"> Apron (laden containers on trailers only) 	20	32	48	50	3
<ul style="list-style-type: none"> Former shed floor 	20	20	20	20	3
D Appleton Dock (tired vehicles only)					
<ul style="list-style-type: none"> Apron (laden containers on trailers only) 	20	32	48	50	3
<ul style="list-style-type: none"> Former shed floor 	20	20	20	30	3
E Appleton Dock					
<ul style="list-style-type: none"> Apron (laden containers on trailers only) 	20	32	48	50	3
<ul style="list-style-type: none"> Stacking area – Laden containers 1 high, empty containers 2 	20	32	48	60	5
F Wharf apron – No load within the following areas: <ul style="list-style-type: none"> Front 1.8 m width of wharf Section of wharf between 19 m and 26 m from the front edge of the wharf Within 6 m of grain shiploader wheels, with the exception of one T44 truck only 	20	32	48	50	3
HOLDEN OIL DOCK					
Pier head & vehicular approaches	9	16.5	20	9	0.5
YARRAVILLE					
5 Wharf apron	Refer to berth operator for details				
6 Wharf apron	This section of wharf is demolished				
<ul style="list-style-type: none"> Chainage 0 to 30 m Chainage 30 to 235 m (Additional restrictions apply, see Notes) 	9	16.5	20	9	NIL
	Bulk cargo hopper 52t gross				
GELLIBRAND PIER					
<ul style="list-style-type: none"> Roadway (Except Approach Ramp) 	9	16.5	20	9	0.9
<ul style="list-style-type: none"> Approach Ramp 	9	13	15	9	0.9
<ul style="list-style-type: none"> Pier head 	14	16.5	20	14	3
BREAKWATER PIER					
<ul style="list-style-type: none"> Chainage 0 to 116 m 	2	2	2	2	NIL



BERTH No./LOCATION	COL. A SINGLE AXLE	COL. B TANDEM AXLE (T)	COL. C TRI- AXLE (T)	COL. D FORK LIFT (T)	COL. E SPREAD LOADS (T) (T/SQ.m)
WEBB DOCK EAST					
1 Wharf apron	10	16.5	20	20	1
1 Stern approach	32	52	52	80	3
2 Wharf apron	24	40	42	34	2
2 Stern approach	32	52	52	50	3
3 Stern approach & wharf	2	2	2	2	0.5
4 Webb Dock East					
• Wharf apron	40	60	60	120	4.5
• Terminal Pavement (between 0 m and 16 m from rear of wharf apron)					3.5
• Terminal Pavement (between 16 m and 62 m from rear of wharf apron)					5
5 Webb Dock East					
• Wharf apron	40	60	60	120	4.5
• Terminal Pavement (between 0 m and 16 m from rear of wharf apron)					3.5
• Terminal Pavement (between 16 m and 62 m from rear of wharf apron)					5
WEBB DOCK WEST					
1 Wharf apron	40	60*	60*	120	5
2 Wharf apron	40	60*	60*	120	5
3 Wharf apron	40	60*	60*	120	5
	* can be increased with restrictions				
SHORT ROAD					
Tug Berths	11.6	11.6	11.6	11.6	1
Wharf	10	10	10	10	1
• Chainage 110 to 190m - Light vehicles (Max 2t) and pedestrian only within 4 m of wharf edge					

Notes:

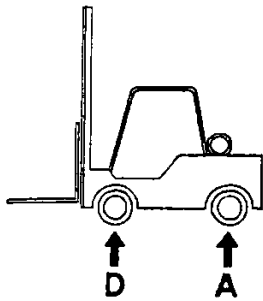
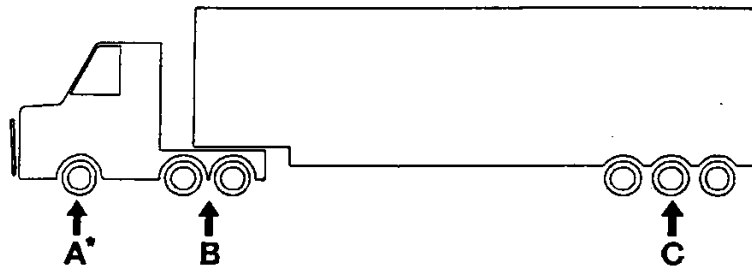
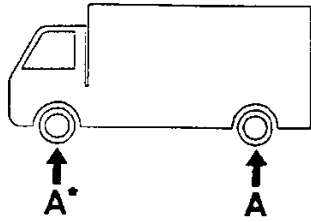
Yarraville 6 Operational Restrictions:

- Only pedestrian and light vehicle loading permitted within 2.8 m of wharf edge
- The storage of materials or equipment on the wharf is strictly prohibited
- Hoppers may not be operated within 10 meters of each other (clear spacing), and only one hopper may be loaded at any time
- Hoppers shall not be re-loaded until the laden truck has departed
- One truck only may be permitted to operate on the wharf at a given time
- Loaded trucks are to depart the facility in a timely manner
- Queuing of trucks is not permitted either on the wharf or within 15m behind the sheet pile wall (rear of wharf apron)



Attachment 1)

Standard Load Diagram



A = SINGLE AXLE

B = TANDEM AXLE

C = TRI-AXLE

D = FORK LIFT

E = UNIFORM LOAD

*** = AXLES WITH ONLY 2 TYRES MAY BE LIMITED - SEE NOTES.**

